

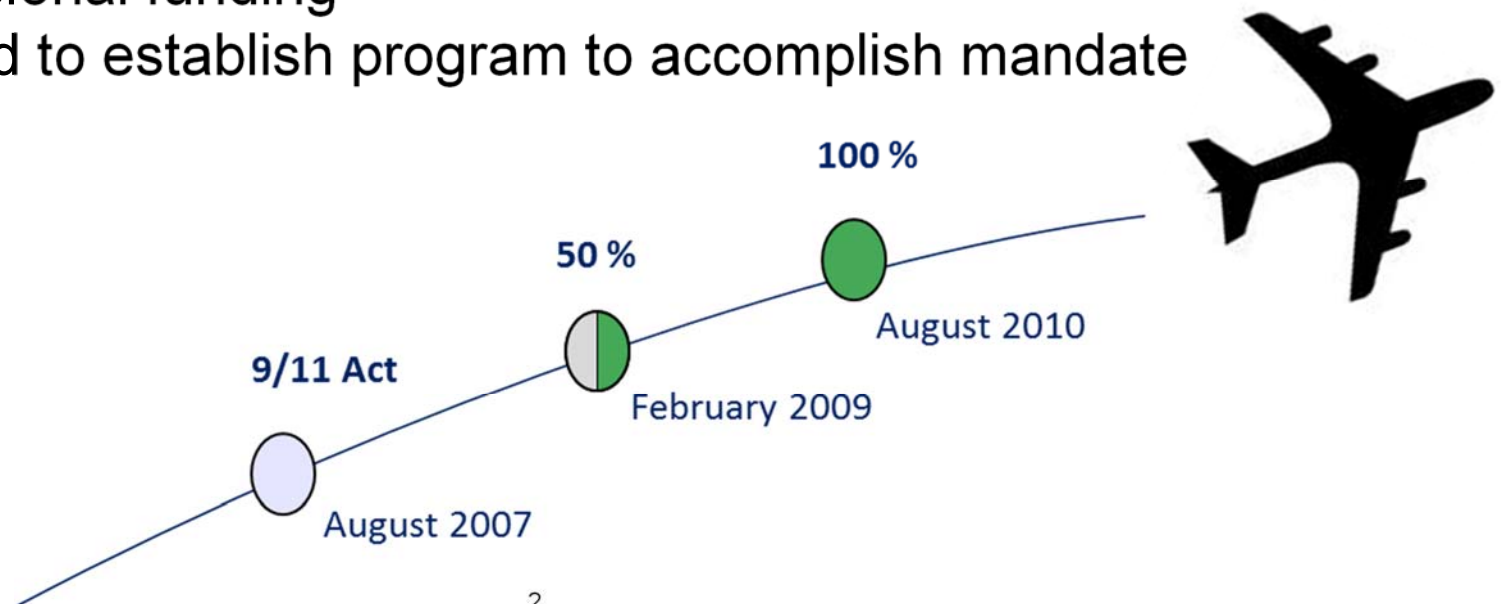
TSA Air Cargo Screening Update



ALERT ADSA16
Cargo Inspection Workshop
2 May 2017

Law – 100% Screening Legislation

- The Implementing Recommendations of the 9/11 Commission Act of 2007 were signed into law on August 3, 2007
- The law required 100% screened for domestic flights by August 3, 2010 at the piece level
- Further guidance required 100% screened for international inbound flights to the United States by December 3, 2012
- Prevent or deter the carriage of any unauthorized persons, and any unauthorized explosives, incendiaries, and other destructive substances or items in cargo onboard an aircraft.
- Commensurate with baggage (piece level)
- No Congressional funding
- TSA required to establish program to accomplish mandate



Challenges to Meeting 100% Screening

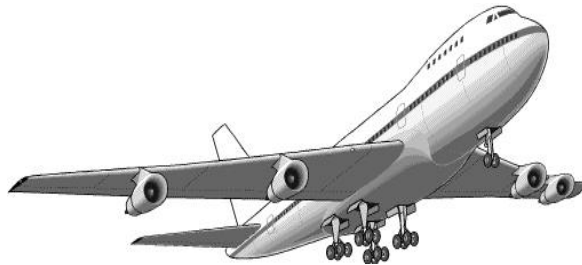
Screening Capability/Volume

- ✓ Airlines at screening capacity
- ✓ 600k-700k pieces each day



Screening Cargo

- ✓ Screen an individual piece within a shipment
- ✓ Cargo could be any size
- ✓ Cargo could contain time-sensitive commodities



Limited Screening Technology

TSA Approved technologies:

- ✓ Physical Search
- ✓ X-Ray
- ✓ Explosives Trace Detection (ETD)
- ✓ Explosives Detection System (EDS)
- ✓ Electronic Metal Detection (EMD)
- ✓ K9 (secondary)

Funding

- ✓ No Congressional funding for screening



Overview of the Air Cargo Supply Chain

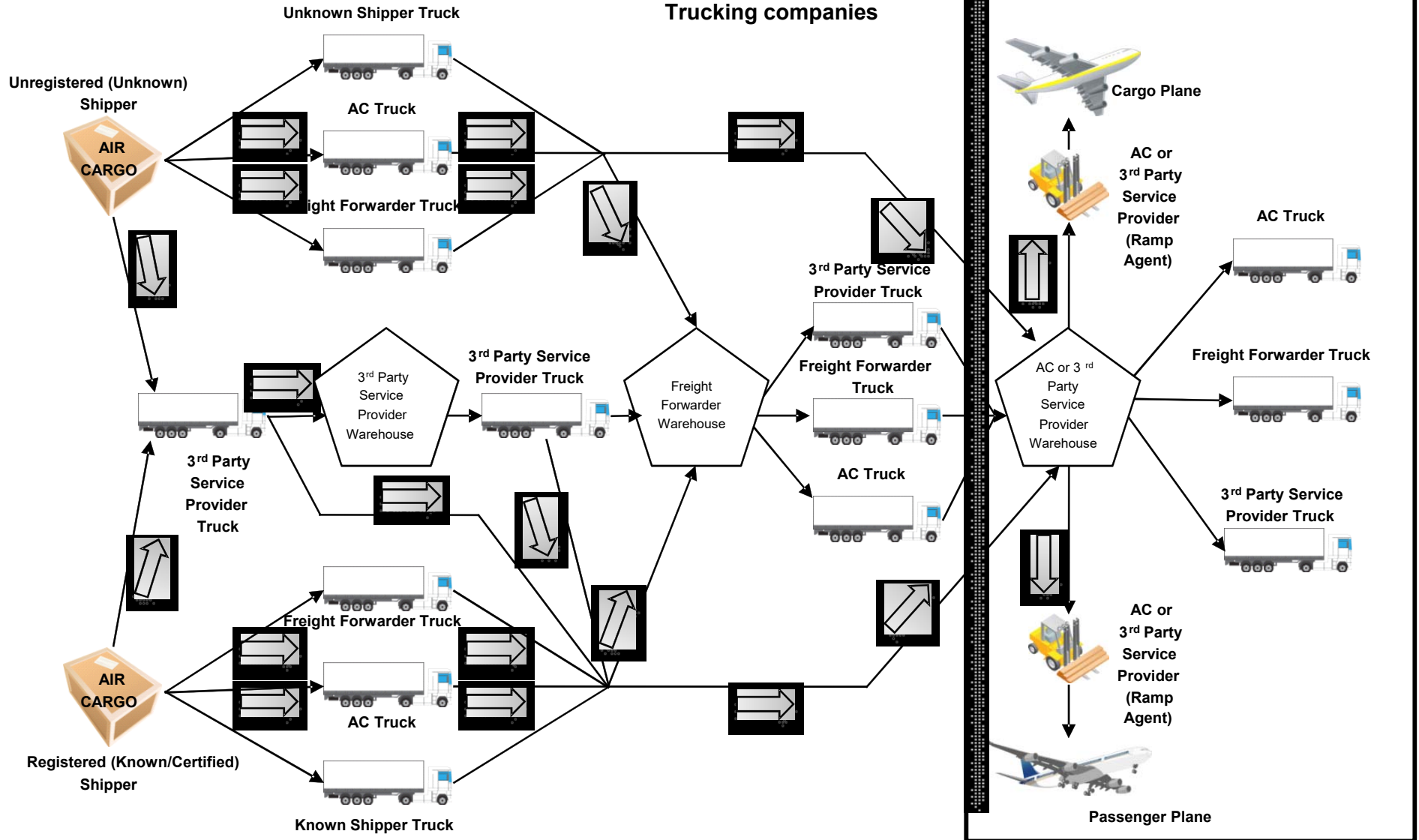


Millions of Shippers

Over 4000 IACs

Over 700,000
Trucking companies

300 Air Carriers at 450 Airports



TSA Domestic Approach for 100% Screening



- TSA established the Certified Cargo Screening Program (CCSP)

CCSP

- Enables all entities in the supply chain who meet stringent security standards to screen cargo
- Businesses may choose the best and most effective screening model for their needs
- Supported and implemented by industry
- Leverages best practices from global supply chain security programs

Approach Includes:

- Standard Security Program updates
- Compliance Inspections/Audits
- TSA Proprietary Canine Teams
- Risk-Based Strategies

Over 2300 Cargo Screening Facilities Across Supply Chain About Half are under the CCSP

Air Carriers

~1200 sites

IACs

~520 CCSF sites

Shippers

~500 CCSF sites

Independents (ICSFs)

~80 CCSF sites

Current Approved Methods of Screening



- The following screening methods are approved for passenger air cargo:
 - Physical Search
 - X-Ray
 - Explosives Trace Detection (ETD)
 - Electronic Metal Detection (EMD)
 - Explosives Detection System (EDS)
 - TSA-Certified Canines

 - CO2 Monitors -- All-Cargo carriers (stowaways)

- Additionally, Sec. 1602 of the 9/11 Act states, “The Administrator may approve additional methods to ensure that the cargo does not pose a threat to transportation security and to assist in meeting the [screening] requirements...”

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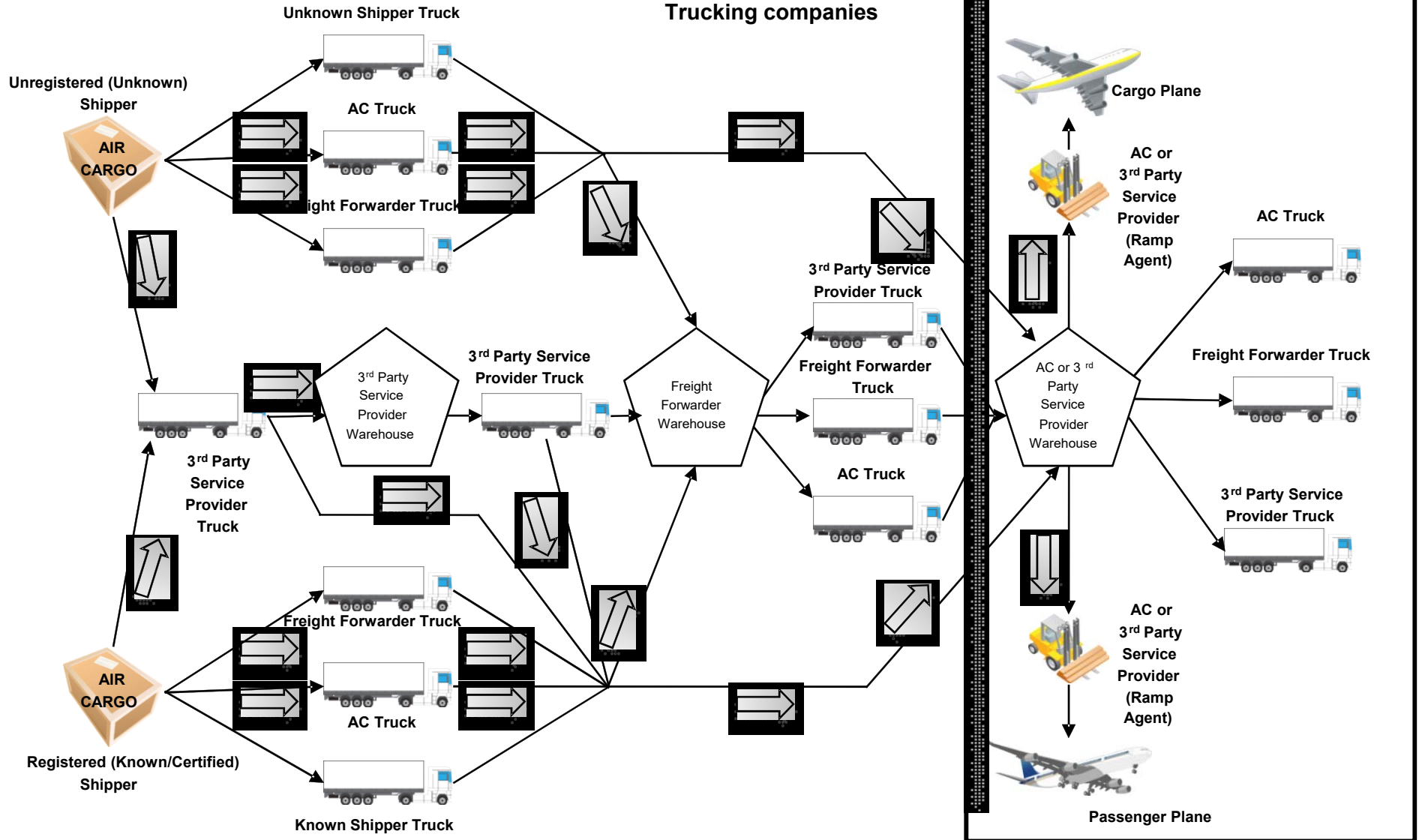


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Tonnage Screened



	2010 Domestic Air Cargo	2016 Domestic Air Cargo
Total	~250M	~300M
Air Carrier	~160M	~100M
IAC/CCSF	~70M	~150M
Shipper/ICSF	~20M	~50M

Key Point:
Today, CCSFs perform
2/3 of domestic
screening



ACSQT Redesign Overview

Rolling Submission Process

- Manufacturers may submit white papers for all technology categories throughout the calendar year
- Manufacturers may resubmit devices 90 days from the date of non-approval notification from TSA (formerly, manufacturers would have to wait till a new submission window opened)

TSA issued this Request for Information (RFI) to announce a redesigned Air Cargo Screening Qualification Test (ACSQT) on FedBizOpps.Gov

- <https://www.fbo.gov/index?s=opportunity&mode=form&id=8e40c67449f21b6ad34e735942026dd0&tab=core&cvview=0>

TSA Air Cargo Screening Technology List (ACSTL)



Currently, there are 119 pieces of technology on the list, version 10.2 dated 12/15/2016.

91 X-ray

- 37 qualified x-ray
- 1 approved
- 53 grandfathered (single view)

3 ETD

- 3 grandfathered ETD

15 EDS

- 15 qualified EDS

8 EMD

- 7 qualified EMD
- 1 approved

2 CO2

- 2 qualified CO2 monitors

- Non-SSI version posted on:

<https://www.tsa.gov/for-industry/cargo-programs>

Estimated Tech Count



Equipment Quantities, as of July 2016

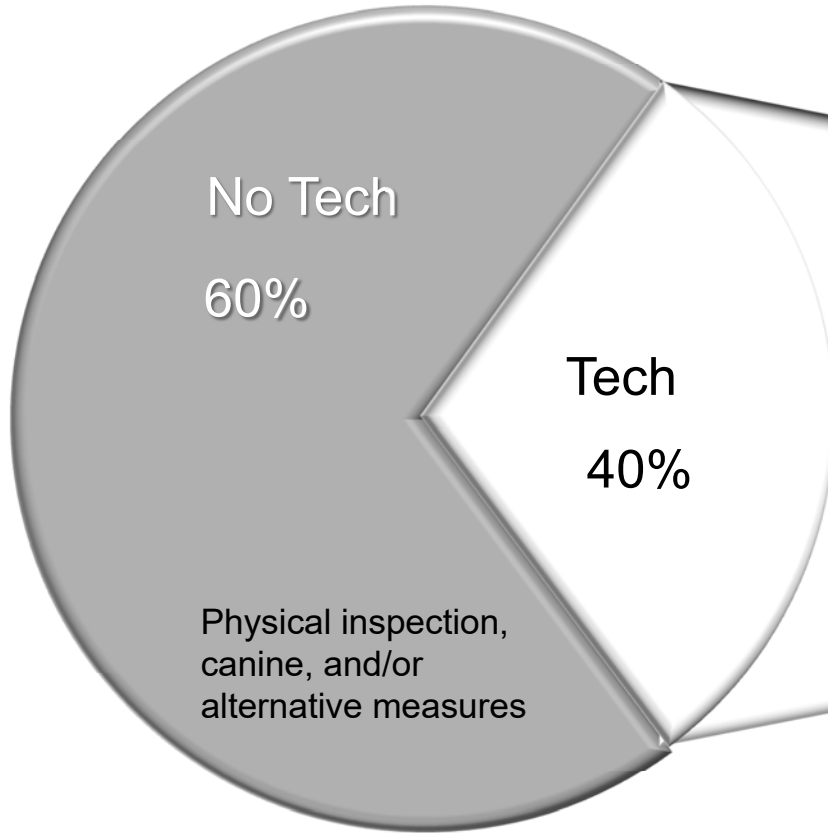
Domestic Air Cargo Population			
	CCSF	Air Carrier	Total
ETD	354	482	836
X-Ray	141	110	251
EMD	16	1	17
EDS	1	2	3
Total	512	595	1,107

Down approximately 150 units from 2012

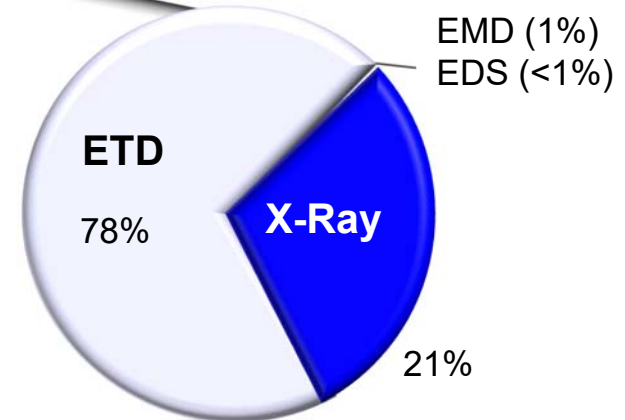
Technology Usage



By Site:



By Unit: Deployed



Total No. of active domestic screening sites: ~2,300
Total No. of Sites using Technology: ~700

International Inbound

- TSA has adopted a two-fold approach to implementing 100% screening for international inbound cargo:
 - Increase screening requirements in the airline Standard Security Programs (SSPs)
 - Recognize commensurate foreign air cargo security programs through the National Cargo Security Program (NCSP) to enable air carriers flying directly into the U.S. to follow only the national cargo security program

- TSA has developing a risk-based strategy based on identifying high-risk cargo for enhanced screening measures
 - Applying knowledge gained from on-going risk assessment and mitigation efforts in domestic air cargo

Focus on All Cargo as a “soft target”

- Aircraft as a weapon/aircraft is the target
- Similar Security Programs
 - Risk based enhanced screening
- Inspectors
- Larger configurations
- Complex cargo
- Larger threat mass

Issues to solve



- COST COST COST
- No silver bullet
- All Cargo screening
- E-Commerce
- Level I detection (EDS)
- Manpower/Human Interpretation

Commodities pose significant screening challenges

Supply Chain Challenges

Screening Challenges

- Requires cold chain handling
- FDA sealed

- X-Ray may affect shipments
- Compromised package integrity

- Requires cold chain handling
- Perishable/limited shelf life
- USDA/APHIS requirements

- Too dense for X-Ray
- Risk of physical search bruising
- Difficult for ETD (wet)

- Sealed drums
- Possibly toxic if opened
- Liquids/powders

- Too dense for X-Ray
- No alarm resolution for ETD
- Inability to physically screen

- Compromised package integrity
- High value security

- Static discharge
- Risk of physical search damage

- Sanctity of the remains

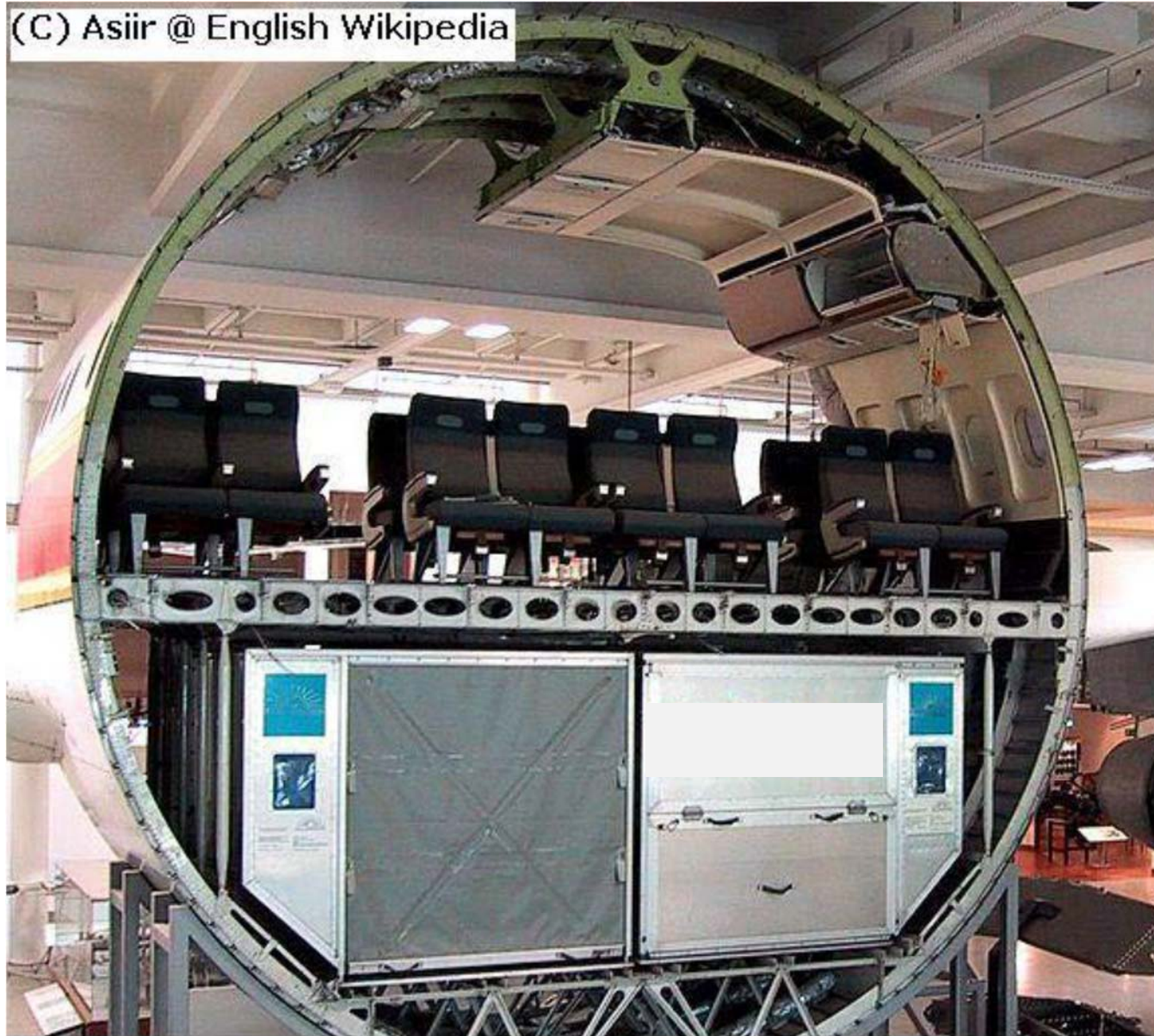
- Inability to physically screen

- Varying sizes
- Sophisticated packaging
- High value

- X-Ray sensitivity
- Inability to physically screen



Do you know what you are sitting on?





Questions?

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